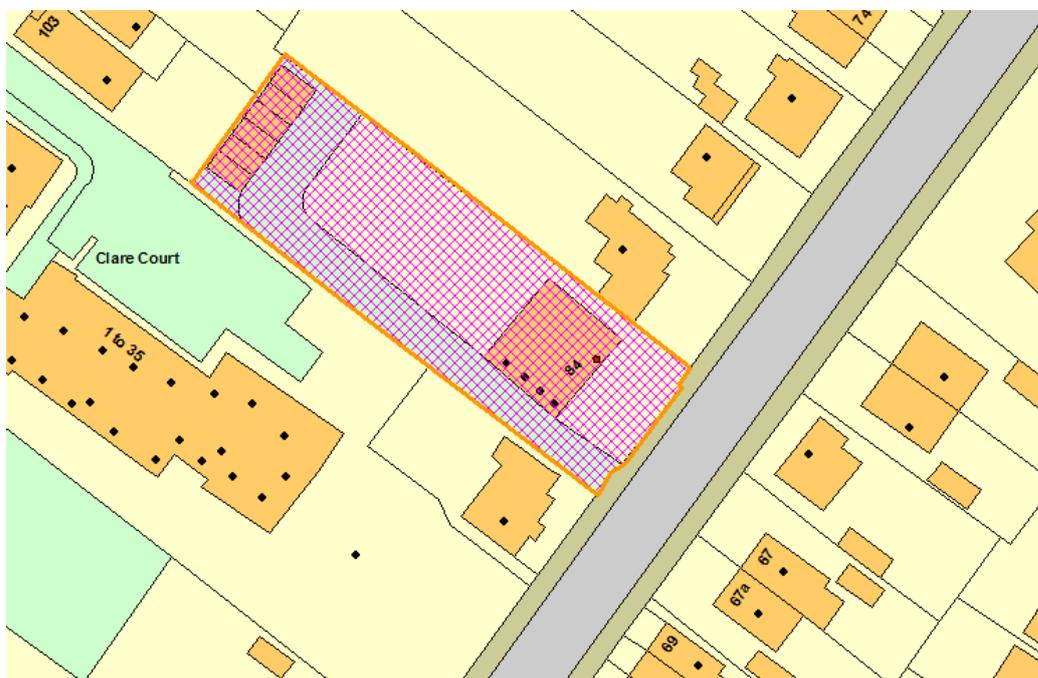


**COMMITTEE REPORT
ITEM NUMBER: 102**

| | |
|----------------------|--|
| APPLICATION NO. | 20/02513/FUL |
| LOCATION | 84 Connaught Road Fleet Hampshire GU51 3LP |
| PROPOSAL | Construction of 2x two-bedroom dwelling houses with associated garages, parking and landscaping (following demolition of existing garage block). |
| APPLICANT | Mr S Mosley |
| CONSULTATIONS EXPIRY | 26 January 2021 |
| APPLICATION EXPIRY | 10 December 2020 |
| WARD | Fleet Central |
| RECOMMENDATION | Grant, subject to planning conditions |



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BACKGROUND

This planning application is brought to Planning Committee at the request of Ward Councillors who have asked it to be referred for the following reasons:

- Unsuitable overdevelopment of a back land garden site;
- Serious impacts on privacy of neighbours due to scale and height of the proposed building.

SITE

The application site is located on the north-western side of Connaught Road, Fleet. The site comprises of a two storey detached building containing four flats and a singled storey block of garages to the rear. There are six garages within the block. Vehicular access to the site is from Connaught Road via a driveway running along the western boundary of the site.

The perimeter of the site features trees of substantial size at either side of the shared boundaries. The area in between the buildings on site is mostly lawn for amenity purposes, however it is also noted that it is used as a car parking area.

The existing two storey building on the frontage of the site is set back from the road and features lawn to its forecourt. There is a small group of trees and other vegetation on the eastern corner of the site frontage which benefits from a TPO designation.

The immediate surrounding area is residential in nature featuring a wide range and sizes of detached, semi-detached and terrace dwellings. The site is a short walk to the Fleet town centre.

SITE DESIGNATIONS

- The site contains a Tree Preservation Order (eastern corner of frontage).
- The site is located within the settlement boundary.
- The site is located within 5Km of the TBHSPA

PROPOSAL

The application seeks planning permission for the erection of 2 no. two-bedroom dwellings, each with an attached garage. The existing garage block would be demolished.

The proposed dwellings would be sited 10m away from the rear boundary and have a rectangular footprint measuring a maximum of 10.3m in depth by 5.6m in width). Whilst the dwellings would be two-storey, the garages would be single storey measuring 3.20m in width by 6.92m in depth.

Both dwellings would have pitched roofs with a maximum ridge height of 8.1m and an eaves height of 4.9m. The single storey garages would have pitched roof with a maximum ridge height of 5.7m and an eaves height of 2.5m. The proposed garage for Plot 2 would have a hipped roof sloping away from the boundary with No. 82 Connaught Road.

Each proposed dwelling would have one car parking space within the garages and one space in front on the driveway. In addition, four parking spaces are proposed to serve the flats that would be retained on site. These would be located to the rear of a new boundary enclosure that would create a private amenity space for the retained flats.

The car parking area and the dwellings would be accessed via the existing vehicular entrance to the site. The only alteration proposed to the entrance is the widening of the hardstanding area to achieve a clear width of 4.8m and the creation of a bin storage/collection point next to it.

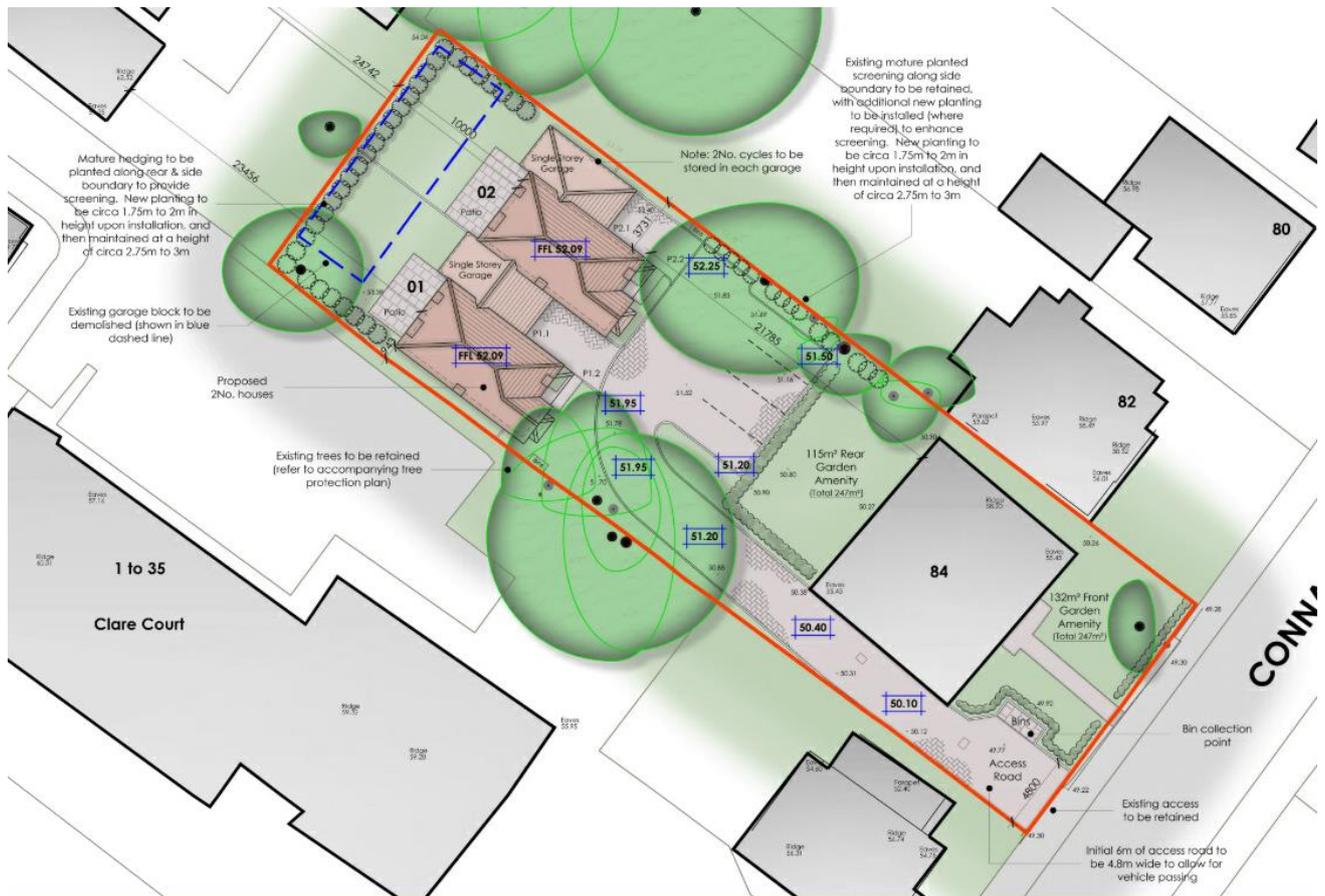
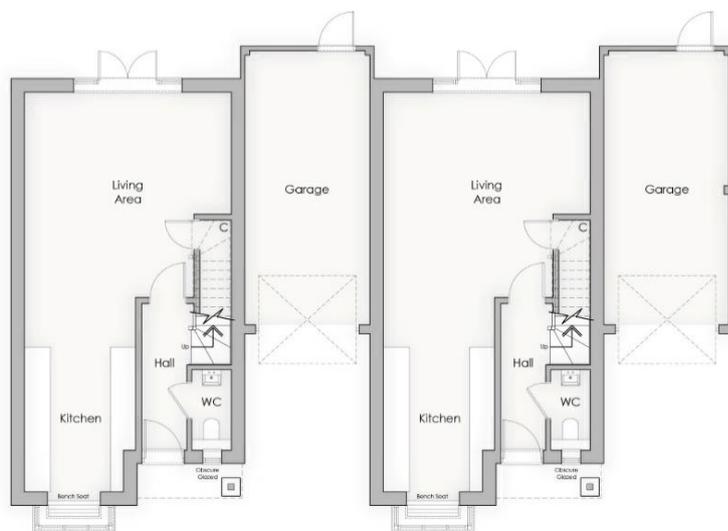
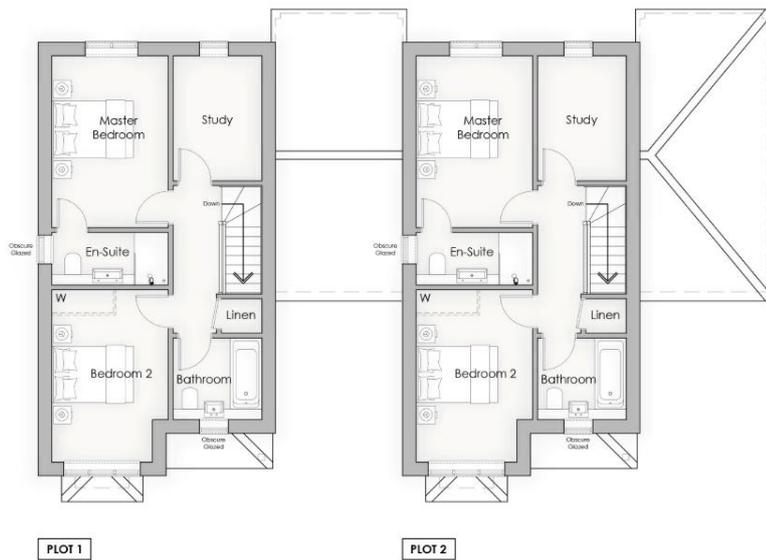


Figure 1 - Site Layout



GROUND FLOOR PLAN
Scale 1:100 @ A2



PLOT 1

PLOT 2

FIRST FLOOR PLAN
Scale 1:100 @ A2

Figure 2 - Floor Plans



PLOT 1

PLOT 2

FRONT ELEVATION
Scale 1:100 @ A2



PLOT 2

PLOT 1

REAR ELEVATION
Scale 1:100 @ A2



Figure 3 - Proposed Elevations

RELEVANT PLANNING HISTORY

19/01498/FUL- Withdrawn, 25.09.2019

Construction of a two-storey building to the rear of the site to accommodate 6 x 1-bedroom flats with associated car/cycle parking and landscaping (following demolition of block of 6 garages).

19/02713/FUL- Withdrawn, 06.02.2020

Three 2-bedroom dwellinghouses with associated parking and landscaping (following demolition of existing garage block to the rear).

20/00840/FUL - Refused, 21.08.2020

Construction of 2x two-bedroom dwellinghouses with associated garages, parking and landscaping (following demolition of existing garage block).

Note: The latest application above was only refused due to the impact of the proposed dwelling on the Thames Basin Heaths Special Protection Area (TBHSPA).

CONSULTEES RESPONSES

Fleet Town Council

Objection.

- 4 existing flats - only 4 parking space provided; how many bedrooms?? Losing 6 garage spaces?
- Areas of no dig construction lifts levels by a min of 100 mm. How is this difference accommodated on site and how does it affect the access to Plot 2?
- Significant increase in impermeable area on plot - where will the SUDS systems be accommodated?
- o Positively this provides two small family houses rather than adding to the oversupply of apartments.

Hampshire County Council (Highways)

No objection, subject to planning conditions

- Construction Management Plan

Tree Officer (Internal)

No objection, subject to conditions.

- Development to be implemented in accordance with the Arboricultural Tree Protection Plan ref: TPP-01 Rev B (dated 9th June 2020).
- Plans for installation of services and means of installation should they conflict with trees.

NEIGHBOUR COMMENTS

It should be noted that the statutory requirements for publicity, as set out in the DMPO 2015 (as amended) are in this case the notification of the adjoining properties or the display of a site notice. In this case the adjoining properties/owners have been notified by post. The Council's SCI has now been amended so that we are only required to carry out the statutory publicity requirements, thus in this case it is not necessary to display a site notice.

Neighbour Letters were sent to neighbouring properties. The 21-day public consultation expired on 13.11. 2020. However, there was an additional consultation to allow for comments on the additional information submitted. At the time of writing the officer's report 21 public representations had been received, 20 of them in objection and 1 neutral. Some objectors submitted more than one representation; hence they have been single counted. The summary of planning related objections is listed below:

- o Overdevelopment of the land.
- o Out of keeping with the character and appearance of the area
- o Overbearing/dominating.
- o Highway impacts (unsuitable access, insufficient parking, pedestrian safety)
- o Noise and disturbance
- o Undesirable precedent to develop garden space (garden-grabbing).
- o Harmful back land development.
- o Loss of amenity space for the existing flats
- o Loss of privacy and overlooking of neighbouring properties.
- o Loss of daylight/sunlight to neighbouring properties.
- o Study areas could become additional bedrooms.
- o The internal floorspace does not meet minimum internal standards.
- o Loss of trees.
- o Refuse storage inadequate, additional bins to be left on street, blocking the pavement.
- o Development does not provide SANG.

A representation has been received from Hampshire Swifts, requesting the provision of swift bricks in the dwellings.

One of the Ward Councillors has objected to the proposal on the grounds of adverse effects to neighbouring properties, dominance, impact on character of the area, traffic and car parking. The objection also states there is no urgent need for housing.

CONSIDERATIONS

1. Planning Policy

2. Principle of Development
3. Design/Character of the Development and the Area
4. Impact on Neighbours' Amenity
5. Quality of the Proposed Residential Accommodation
6. Trees
7. Thames Basin Heaths Special Protection Area
8. Flooding and Drainage
9. Access and Parking
10. Refuse
11. Other Matters

1. PLANNING POLICY

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise.

The relevant plan for Hart District is the Hart Local Plan: Strategy and Sites 2016-2032 (HLP32), the saved policies of the Hart District Local Plan (Replacement) 1996-2006 (HLP06), saved policy NRM6 of the South East Plan and the Fleet Neighbourhood Plan 2018-2032 (FNP). Adopted and saved policies are up-to-date and consistent with the NPPF (2019).

Hart Local Plan - Strategy and Sites 2016-2032 - (HLP32)

- SD1 - Sustainable Development
- SS1 - Spatial Strategy and Distribution of Growth
- H1 - Housing Mix
- H6 - Internal Space Standards for new Homes
- NBE2 - Landscape
- NBE3 - Thames Basin Heath Special Protection Area
- NBE4 - Biodiversity
- NBE5 - Managing Flood Risk
- NBE9 - Design
- NBE11 - Pollution
- INF3 - Transport

Saved Policies of the Hart District Council Development Plan (Replacement) 1996 - 2006 - (HDP 06)

- GEN1 - General Policy for Development
- CON8 - Trees, Woodland and Hedgerows: Amenity Value

South East Plan

Saved NRM6 - Thames Basin Heath Special Protection Area

Fleet Neighbourhood Plan 2018-2032 (FNP)

- Policy 10 - General Design Management Policy
- Policy 10A - Design Management Policy related to Character Areas
- Policy 17 - Thames Basin Heath Special Protection Area Mitigation
- Policy 19 - Residential Parking

Other relevant material considerations

National Planning Policy Framework 2019 (NPPF)

Planning Practice Guidance (PPG)

Technical Housing Standards - nationally described space standard (DCLG 2015)

BRE Report - Site layout planning for daylight and sunlight: a guide to good practice (2011)

Parking Provision Interim Guidance (2008)

2. PRINCIPLE OF DEVELOPMENT

The application site is located within the urban settlement boundary of Fleet as defined by the HLP32. Policy SD1 is a general policy that states that when considering planning applications, the Council will apply the presumption in favour of sustainable development; this policy mirrors the requirements of the NPPF.

Policy SS1 sets out that development will be focused within the defined settlement boundaries as well as on previously developed land. The policy goes on to set out the Council's Housing requirement and indicate that this will in part be delivered through development or redevelopment within settlement boundaries. The supporting text makes it clear that some of the Council's housing requirements will be delivered through windfall sites such as the application site.

In light of the above policy context the principle of the development of this site is acceptable. However, in order to determine if the detailed proposal is acceptable, it is necessary to consider the detail of the application and fully assess the proposals against the Development Plan as a whole.

3. DESIGN/CHARACTER OF THE DEVELOPMENT AND THE AREA

HLP32 Policy NBE9 seeks to ensure that development achieves a high-quality design and that it would positively contribute to the overall character of the area. The NPPF also reinforces the need to promote good design in developments and states that development should be refused for poor design (para. 130). Policy 10 of the FNP also supports good design that respects the character areas as identified in the Council's Urban Design and Density Study.

The proposed dwellings would be located to the rear of the existing block of flats that fronts onto Connaught Road. In that regard the site is a typical "backland" development. The Council's Urban Design and Density Study (2010) describes this area as comprising "Victorian/Edwardian residential development" (Area E). The original grid pattern of streets that was set out in the early 20th Century is still evident. Whilst the prevailing character is this grid form there is some amalgamation of sites including at Clare Court which is adjacent to the application site. Given the existing building at Clare Court which would be adjacent to the proposed dwellings, the construction of the proposed dwellings would not be unduly harmful to the overall character of this part of Fleet. It is noted that concerns have been raised that by allowing a backland development on this a precedent would be set for other properties to have similar development in the rear gardens. Should future applications be submitted they would be assessed on their own merits, future applications would not be acceptable simply because a different scheme on a different plot was considered appropriate.



Figure 4 - Proposed dwellings, gardens and parking.

There are level changes across the site with the land being higher towards the rear of the site than at street level. Cross sections of the site have been provided that show the proposed dwelling as well as the existing building at No. 84 and 101 Clarence Road. The developer would alter the ground levels within the site in order to create a flat area within which to construct the dwelling and as a result it is unlikely that the ridge would be visible above the ridgeline of No. 84 when stood on Connaught Road. There would be views of the properties when stood at the bottom of the driveway however these would not be unacceptable within the street scene.



Figure 5 - Site's access & view towards the rear

The properties along Connaught Road have a variety of different materials and designs as well as being of different scales although they are all typically two storey buildings with some bungalows. The proposed dwellings have been designed sympathetically in terms of their design features and the palette of materials. In this regard they would be in keeping with the character of the area.

In terms of scale, the properties would display acceptable proportions and an appropriate width/depth ratio. The roof profile proposed with a sloping roof and hip ends would contribute to moderating the overall scale of the dwellings.

The single storey garage for Plot 1 would appear to have a high ridgeline as seen from the front, however it would be largely obscured between the properties and its set back from their frontage.

The other single storey garage, sited near the shared boundary with no.82 Connaught Road, would display a hipped end to reduce the bulk of the roof. The scale of the properties would be acceptable, particularly when compared to other two-storey properties in the locality.

The dwellings/garages footprint and shape would largely make use of the full width of the site as there would only be a set-in from the side boundaries of between 0.70m-0.95m. The dwellings would be located 10m away from the rear boundary and there would be a distance ranging between 20.6m and 21.7m from the rear elevation of the existing building standing on the site.



PLOT 1

PLOT 2

FRONT ELEVATION

Scale 1:100 @ A2

Figure 6 - Dwellings South Elevation (main).





Figure 7 - Residential designs along Connaught Rd

Given the above, it is considered that the character and design of the proposed development would respect the character and appearance of the streetscene.

Therefore, the proposal would be in accordance with policies NBE9 of the adopted HLP32, policy 10 of the FNP and the NPPF in terms of design, character and appearance of the dwellings and the area as a whole.

4. IMPACT ON NEIGHBOURS' AMENITY

The neighbouring properties that adjoin the site and are the closest to the proposed development are nos. 82 and 86 Connaught Road, 99-103 Clarence Road and Claire Court (retirement housing) and the existing residential building on the site (84 Connaught Road).

- Nos. 82 and 86 Connaught Road

No.82 adjoins the existing flatted development on site to the east. The distance between this adjoining property and the proposed dwellings would be over 21m and they would not be directly facing each other. It is noted there are mature trees along the shared boundary that would serve as screening between them. A site visit revealed intervisibility between the site and this adjoining neighbour slightly opens up during the winter but no clear views between the development and this adjoining dwelling would be achieved. Thus, neither the privacy, daylight nor the outlook of this property would be materially affected.



Figure 8 – View from the site towards no. 82 (trees left hand side along the shared boundary) and rear of no.84 Connaught Road.

The only impact anticipated to no. 82 arises as a result of the siting of the development in proximity to the shared boundary and the northern portion of its rear garden. The proposed dwelling - Plot 2 (two storey) would be 3.73m away from the boundary with this neighbour and the single storey garage would be 0.7m away. The development would therefore be visible from the rear garden of this property behind the planting along the boundary. However, the two-storey property would be set in from the boundary and the single storey garage would feature a hipped end roof sloping away reducing any impacts. The garden would largely retain its open views and character. As such there would be no undue loss of amenity for the occupiers of No. 82 Connaught Road.



Figure 9 - View towards northern portion of the rear garden of no. 82 Connaught Road

With regards to no. 86, this is located in close proximity to the side boundary of the site by

the vehicular access as if fronts onto Connaught Road. There would be an increase in the number of vehicles using this driveway which would create the potential for additional noise and disturbance.



Figure 10 - 86 Connaught Road (white building), access to the site.

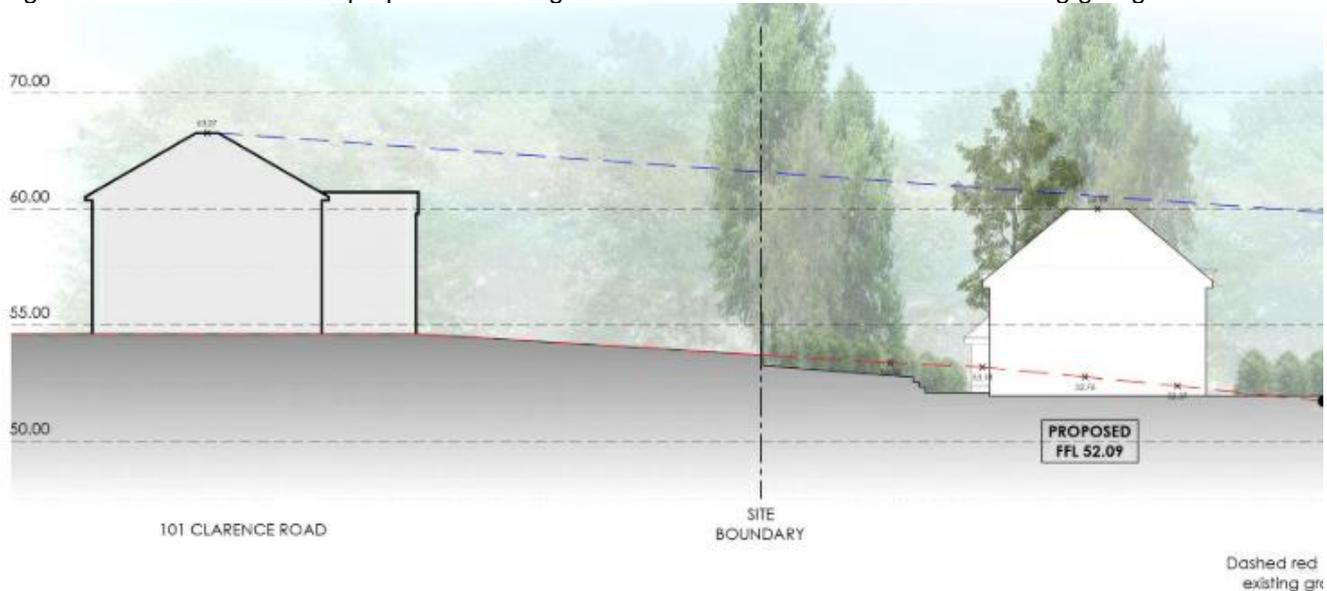
No. 86 is also set in from this shared boundary (over 3m away) and a single storey garage stands between this adjoining property and the shared boundary. The flank elevation of this adjoining dwelling features a narrow upper-level window that serves a bathroom. Main habitable spaces are served by windows in the front/rear elevations. Therefore, the movements associated with two additional dwellings would not create material impacts on their residential amenity. No other impacts would arise from the development in respect of this adjoining property.

- Nos. 99 -103 Clarence Road

These properties are located to the rear of the application site. At the closest, the proposed dwellings would be 23.45m away from these three existing dwellings and the proposed dwellings would have a garden depth of 10m. It should be noted, as previously stated, that ground levels rise towards Clarence Road, so these existing properties would be on higher ground (almost 5m higher) when compared to the ground level where the new dwellings are proposed.



Figure 11 - Rear elevation of properties fronting onto Clarence Road visible above existing garages.



Dashed red
existing gr

SITE SECTION A-A

Scale 1:200 @ A2

Figure 12 - Site Section showing relationship between proposal and Clarence Road properties.

The shared rear boundary between the site and these existing dwellings does not have mature vegetation, however given the level changes views between buildings would only be achieved from first floor windows. However, the separation distances are reasonable and exceed the rule of thumb distances of 20-22m. Some Landscaping is proposed along the rear boundary which would further soften the views. Given the above, it is considered that the proposal would not cause undue harm to the amenity of the occupiers of Nos. 99-103 Clarence Road.

- Claire Court (retirement housing)

This large development is located to the south-west of the application site. The orientation of Clare Court is such that there is habitable accommodation along the flank elevation facing

into the application site.



Figure 13 - View of Claire Court from the site.

The dwelling on Plot 1 would be sited 0.95m away from the shared boundary with Clare Court and there would be a separation distance of between 14-18m between the two buildings. There is a single window proposed on this side elevation that would serve an ensuite; a planning condition can ensure that this would be fixed shut and obscured glazed

Given the separation distance and the fact that the only window would be obscure glazed there would not be any loss of privacy. Plot 1 would be adjacent to the car park of Clare Court and as such would not appear overbearing when viewed from any of the amenity areas serving that development.

It is also noted there is vegetation along the shared boundary between these two sites. The vegetation is varied as there are sections with plants of modest height (1.5m -2m) and there are also mature trees with large canopies, which would soften the change that would be created by the proposal, as visible in Figure 13 above.

- 84 Connaught Road (existing flatted development on site)

Finally, the existing flatted development on site would be the one property experiencing the biggest change as result of the proposal as they would be sited directly opposite each other.

The applicant has submitted site sections with finished ground levels which depict the relationship between the proposal and existing dwellings on site. The proposed dwellings would sit on ground that is on average 1.84m higher when compared to the ground adjoining the rear elevation of the existing building.



Figure 144 - Existing Rear Elevation of 84 Connaught Road.

However, the proposed houses would be sited between 21.7m and 20.6m away from the rear elevation of the existing building on site, which is regarded as acceptable to maintain satisfactory levels of privacy between buildings. The sectional drawing submitted also shows the eaves height of the proposed dwellings would only be 1.37m higher when compared to the eaves of the existing building on site and the pitched roof proposed for the dwellings would slope away to reduce the impact of the dwellings and avoid enclosure/overbearing effects.

Despite the proposal being on higher ground, it would not interfere with the 25° vertical plane line when drawn from the cill of the rear windows of the existing building; and in any event the proposed dwellings would be sited to the north. As such no impacts on daylight/sunlight would be experienced by the windows in the rear elevation of the existing building.



Figure 15 - Site Section showing relationship between proposal and 84 Connaught Road.

The overlooking of amenity space between buildings from one upper-level window serving each dwelling would not be out of the ordinary in an urban area such as Fleet, therefore it would not raise concerns.

It is noted that the outdoor amenity space for this existing development would be reduced as a result of the proposal, but it would still maintain 115sqm of garden area. Additionally, Oakley Park is 500m from the application site, which would complement the on-site outdoor amenity area provision.

In this instance, given the backland nature of the development, the site constraints and relationship with neighbouring properties the restriction of permitted development rights is required to allow the Planning Authority to properly consider the potential effects of any future extension or alterations on the amenity of neighbouring properties.

Given the above analysis, the proposal would not create impacts on residential amenity that would amount to being detrimental to the living conditions of occupiers of surrounding dwellings, as such the proposal is in compliance with policy NBE9 the adopted HLP32, saved policy GEN1 of the HLP06, policy 10 of the FNP and the NPPF in this regard.

5. QUALITY OF THE PROPOSED RESIDENTIAL ACCOMMODATION

The Council has adopted the Nationally Described Space Standards for dwellings in the HLP32. The space standards set out the minimum gross internal floor areas for dwellings as well as requiring certain minimum sizes of bedrooms. The proposed dwellings would comply with these minimum standards.

Concern has been raised that the proposed dwellings would be capable of accommodating three bedrooms as the “study” would be capable of accommodating a single bed.

The NDSS state that the minimum requirements for a single bedroom are a floor area of at least 7.5sqm and a width of at least 2.15m. The proposed study would have an area of 6.2sqm and a width of 2.07m.

As such, the study area would not comply with the minimum requirements set out in the NDSS to count as a bedroom, therefore the proposal has to be assessed as for two-storey 2-bedroom dwellings. Therefore, the room sizes would comply with the minimum space requirements.

Externally, gardens are proposed to the rear of the properties for the amenity purposes of prospective occupiers; these would have a depth of 10m. Hart has not adopted any minimum garden sizes therefore it necessary to consider the National guidance. The general requirement set out in the Government’s design guidance is that the outdoor amenity areas for new dwellings should reflect the footprint of the property, which in this case is around 50sqm. The area of the gardens to be provided would be between 85sqm - 87sqm, which would be sufficient to satisfy outdoor amenity requirements. Properties in the locality benefit from rear gardens of different sizes, some are similar in area to those proposed in this application. The size of the proposed rear gardens are therefore acceptable.

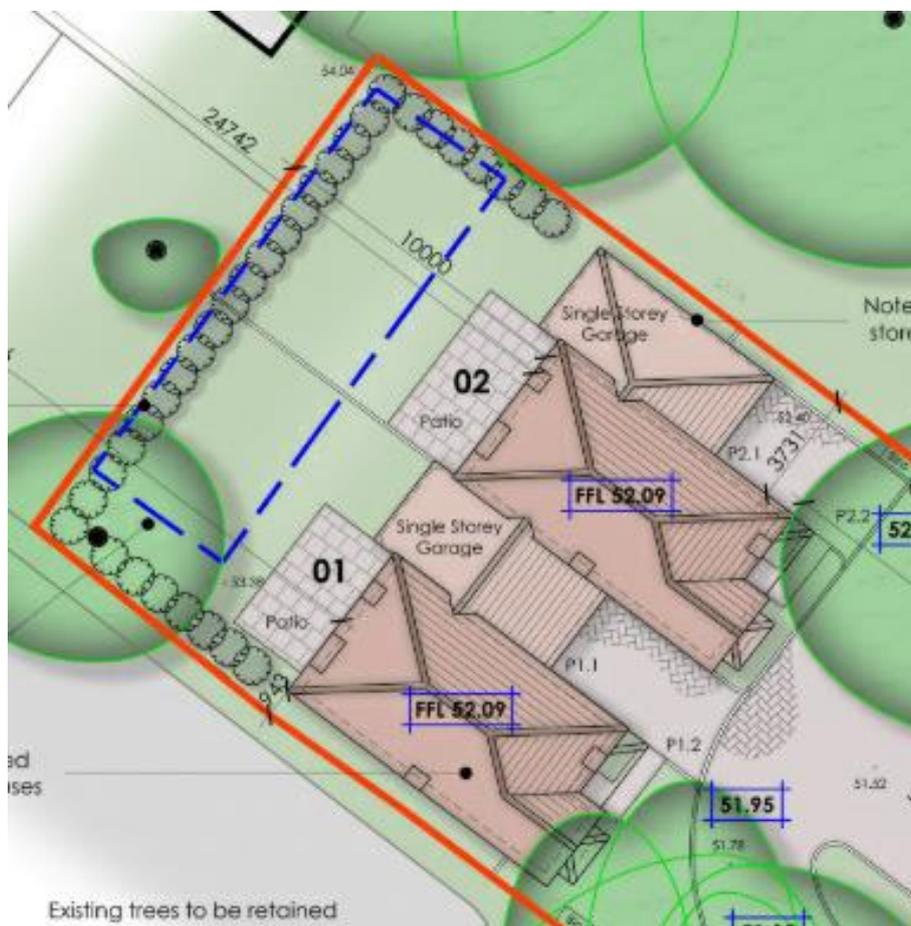


Figure 156 - Rear gardens and forecourt of dwellings.

As such the development would be acceptable in this regard and in compliance with the objectives of policy NBE9 of the HLP32, saved policy GEN1 of HLP06, policy 10 of the FNP and the NPPF.

6. TREES

Concern has been raised regarding the loss of trees as a result of the proposed dwellings. The Council's Tree Officer has considered the application and the Arboricultural report and raised no objections. Whilst there would be some tree felling there are several trees that would be retained, and it would be necessary to ensure that these are protected during the construction phase; this can be dealt with by planning condition.

As such the development would be acceptable in terms of tree impacts and in compliance with the objectives of saved policy CON8 of the HLP06, and the NPPF, subject to conditions.

7. THAMES BASIN HEATHS SPECIAL PROTECTION AREA

The Habitats Regulations 2017 requires Local Planning Authorities (as the Competent Authority) to consider the potential impact that a development may have on an European Protected Site. In this case this relates to the Thames Basins Heaths Special Protection Area (TBHSPA).

The TBHSPA is a network of heathland sites which are designated for their ability to provide a habitat for the internationally important bird species of woodlark, nightjar and Dartford warbler. The area is designated as a result of the Birds Directive and the European Habitats

Directive and protected in the UK under the provisions set out in the Habitats Regulations. These bird species are particularly subject to disturbance from walkers, dog walkers and cat predation because they nest on or near the ground.

Natural England has indicated that it believes that within 5km of the SPA additional residential development in combination will have a significant effect on the SPA. Thus, without avoidance measures any proposal is contrary to the Conservation of Habitats and Species Regulations 2017. In this instance, the application site falls within the 5km Zone of Influence of the SPA, as such avoidance measures are required.

In this instance, the applicant has addressed the potential negative effects on the SPA by securing access to Council's owned SANG and an associated payment towards SAMM has been secured. It is therefore possible to conclude that the proposed development would not have any adverse effect on the integrity of the TBHSPA.

Consequently, the application would be in compliance with policy NBE3 of the HLP32, policy 17 of the FNP, saved policy NRM6 of the South East Plan and the NPPF in this regard.

8. FLOODING AND DRAINAGE

The site is located in a Flood Zone 1 location where there are low risks of flooding but there is potentially surface water flooding towards the front of the site. Thus, the main matters to address in this regard would be provision of suitable surface water drainage to deal with surface water as a result of the development and also to avoid run-off to adjoining sites and existing development on the site.

The proposal would increase hardstanding areas on site; therefore, the principles of a surface water strategy have been submitted to appropriately deal with these matters. The information submitted states that the ground is underlain by Camberley Site Formation which would have a suitable permeability to support inclusion of soakaways.

Areas of hardstanding (access, parking area and drives) would feature permeable materials which would allow surface water run off to drain through paviour gaps and infiltrate to the ground. Geocellular crate soakaways (outside root protection areas) would be provided in the rear garden of the dwellings and also in the car parking area. The principles of such a strategy are acceptable; however, it would be necessary to have full details of infiltration capacity and technical details of the strategy, which can be secured by planning conditions.

Consequently, subject to planning conditions in this regard, the proposed would be in compliance with policy NBE5 of the HLP32 and the NPPF.

9. ACCESS AND PARKING

The access to the site would utilise the existing arrangement and would not be modified. There is a modification proposed to the internal road as there is a section adjoining the front boundary which would be increased in width to allow two vehicles to pass each other and in this manner avoid vehicles reversing within the site or onto the public highway.

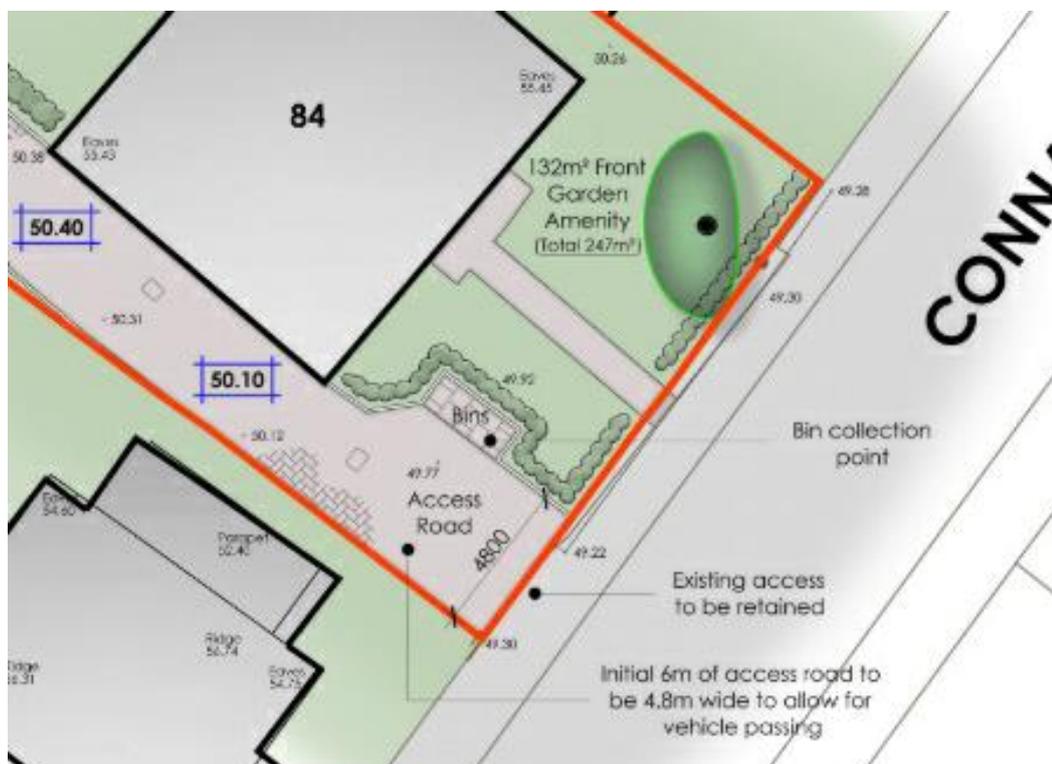


Figure 17 - Proposed increased width to access.

In terms of car parking provision, the two additional dwellings would require a maximum provision of 5 car parking spaces (2 for each dwelling and one visitor space) under the Council's current parking requirements. The proposal also provides 4 car parking spaces for the existing 4 flats in the existing building and according to the interim standards the maximum provision for the existing flats should be 10 spaces (2 each plus 2 visitors).

When assessed against the Council's Interim Parking Standards, there would be a shortfall of 5 parking spaces; this includes the three visitor spaces that would be required. However, these are maximum standards, and it is necessary to consider if a shortfall would cause harm to the safety of the public highway. It is also necessary to consider the location of the development in relation to the Town Centre in terms of accessibility/sustainability.

The site is in a sustainable location as it is a short walking distance from the town centre where there are a range of services and facilities along with access to public transport. It is important to be mindful of the recent appeal decision at 141-145 Clarence Road (our reference 18/01401/FUL) where the Inspector considered that site to be in a sustainable location and whilst dismissing the appeal it was not due to the lack of parking.

Hampshire Highways has not raised any objection to the application and notes that the existing on-street parking controls would remove the risk of obstructive highway parking. It is recommended that a condition requiring a construction management plan to be submitted and agreed prior to development commencing on site to ensure that construction traffic/parking is adequately dealt with.

Therefore, given the level of car parking provision on site, current parking conditions along Connaught Road, the sustainable location of the site and the no objection from the Local Highway Authority; this development would neither result in detrimental impacts to local highways nor would it conflict with policy INF3 of the adopted HLP32, saved policy GEN1 of the HLP06, policy 19 of the FNP and the NPPF.

10. REFUSE

Adequate provision has been made for the collection of refuse which could also serve as a storage area, however, no details of how the refuse storage for the existing development on site along with the proposed development would be stored and managed. Therefore, a condition to secure details of refuse/recycling storage and management for the existing and proposed development is recommended to be imposed if this application is supported by the Council. The Council's Refuse team has been consulted and hasn't raised any objections to the proposal. The proposal is therefore acceptable in this regard.

11. OTHER MATTERS

Concerns have been raised by representations received about the existence of Japanese Knotweed within the site. However, this is not a planning matter. Japanese Knotweed is classified as a controlled plant under the Wildlife and Countryside Act 1981 section 114 (2) (WCA 1981). It is not illegal for landowners to have Japanese knotweed on their property/land. However, it is against UK law to cause or allow the plant to spread. The landowner/developer would have to approach the Environment Agency separately to deal with any necessary removal of this non-native species.

CONCLUSION

The Development Plan Policies and the NPPF requires that sustainable development be approved without delay. The application proposal complies with the requirements of the principal housing policies of the Development Plan and the site is considered to be sustainable and accessible in terms of its location close to Fleet Town Centre. The provision of additional housing is a significant benefit and this development would help meet that need through the delivery of a windfall site.

The design of the proposed development is acceptable and there would be no unacceptable impacts on neighbouring amenity or to the character and appearance of the street scene. Whilst levels of parking provision would be below the Council's maximum standards, given the existing parking controls and the sustainable nature of the location, there would be no adverse impact on highway safety. The proposal would also comply with the requirements of the Development Plan and Habitats Regulations in relation to the TBHSPA.

Given the above, the proposed development complies with the Adopted HLP32, FNP and with the NPPF and is therefore acceptable subject to conditions as detailed below.

RECOMMENDATION – Grant, subject to planning conditions.

CONDITIONS

- 1 The development hereby permitted shall be begun before the expiration of one year from the date of this permission.

REASON: To comply with Section 91 of the Town and Country Planning Act 1990 (as amended) and to prioritise delivery of housing given the limited supply of SANG within the District.

- 2 The development hereby permitted shall be fully implemented in accordance with the

following plans/documents (including any mitigation/enhancement recommended therein):

P19/04/S/401 Rev. B (Site Layout & Roof Plan), P19/04/S/402 Rev. A (Site Sections), P19/04/S/410 Rev. A (Floor Plans and Elevations), TPP-01 Rev. B (Tree constraints/protection plan); and

Arboricultural Impact Statement (dated June 2020) produced by Arbor Cultural Ltd, Levels and Drainage Strategy (dated December 2020) produced by Structa, Design and Access Statement (dated October 2020) produced by Ark Tec.

REASON: To ensure that the development is carried out in accordance with the approved details and in the interest of proper planning.

- 3 The development hereby approved shall not be commenced until details of a site construction method statement and management plan shall be submitted to and approved in writing by the Local Planning Authority and shall include but not limited to the following:

- i) Construction worker and visitor parking;
- ii) anticipated number, frequency and size of construction vehicles;
- iii) dust and Noise/Vibration mitigation measures;
- iv) dust suppression measures;
- v) Site security;
- vi) vehicle manoeuvring and turning;
- vii) locations for the loading/unloading and storage of plant, building materials and construction debris and contractors offices;
- viii) procedures for on-site contractors to deal with complaints from local residents;
- ix) measures to mitigate impacts on neighbouring highways; and
- x) details of wheel water spraying facilities;
- xi) Protection of pedestrian routes during construction

Such details shall be fully implemented and retained for the duration of the works.

REASON: To protect the amenity of local residents, to ensure adequate highway and site safety in accordance with policies NBE11 and INF3 of the adopted Hart Local Plan - Strategy and Sites 2016-2032 and the NPPF 2019.

- 4 No development shall commence (except for demolition of the garages) until a detailed surface water drainage scheme for the site, based on sustainable drainage principles, including an assessment of the hydrological and hydro geological conditions of the site and measures to prevent surface water flooding to the interior of the buildings hereby approved and existing buildings on site has been submitted to an approved in writing by the local planning authority.

The scheme shall be fully implemented in accordance with the approved details before first occupation of the development.

REASON: In the interest of preventing on/off-site surface water flood risk and to satisfy policy NBE5 of the adopted Hart Local Plan - Strategy and Sites 2016-2032 and the NPPF 2019.

- 5 No development shall raise above slab level, until details of all external materials for

the buildings hereby approved are submitted to and approved in writing by the Local Planning Authority. The details approved shall be fully implemented.

REASON: To ensure a high-quality external appearance of the buildings and to satisfy policy NBE9 of the adopted Hart Local Plan and Sites 2016-2032, saved local policy GEN1 of the Hart District Local Plan 1996-2006, policy 10 of the Fleet Neighbourhood Plan 2014-2032 and the NPPF 2019.

- 6 Notwithstanding any information submitted with this application, details of refuse storage and management for both the existing development on site and the development hereby approved shall be submitted to and approved in writing by the Local Planning Authority prior to first occupation. The details approved shall be fully implemented in perpetuity.

REASON: In the interest of an adequate refuse storage and servicing, in accordance with policy NBE9 of the adopted Hart Local Plan and Sites 2016-203, saved local policy GEN1 of the Hart District Local Plan 1996-2006 and the NPPF 2019.

- 7 The approved alterations to the driveway, car parking and manoeuvring areas serving the development hereby approved shall be fully completed and made available prior to the first occupation of the buildings hereby approved. These vehicular facilities shall be retained for these purposes thereafter and access shall be maintained at all times to allow them to be used as such.

REASON: To ensure that the development is provided with adequate access, parking and turning areas in the interest of public highway safety and to satisfy policy INF3 of the adopted Hart Local Plan and Sites 2016-2032, saved local policy GEN1 and the NPPF 2019.

- 8 No development, demolition work or delivery of materials shall take place at the site except between 08:00 hours to 18:00 hours on weekdays or 08:00 to 13:00 hours Saturdays. No development, demolition/construction work or deliveries of materials shall take place on Sundays or Public Holidays.

REASON: To protect the residential amenity of adjoining/nearby residential occupiers and to satisfy policy NBE11 of the adopted Hart Local Plan and Sites 2016-203, saved local policy GEN1 of the Hart District Local Plan 1996-2006 and the NPPF 2019.

- 9 Notwithstanding the provisions of Classes A, B, C, D E and F of Part 1 of Schedule 2 to the Town and Country Planning (General Permitted Development) Order 2015 (or any subsequent order revoking and re-enacting that Order with or without modifications), no enlargement, improvement or other alteration to the dwellinghouses hereby approved, under these classes shall be carried out without the prior permission of the Local Planning Authority, obtained through the submission of a planning application.

REASON: To prevent the overdevelopment of their curtilage to safeguard the living conditions of adjoining properties and visual appearance of the buildings and the area as a whole and to satisfy policy NBE9 of the adopted Hart Local Plan and Sites 2016-2032, saved local policy GEN1, of the Hart District Local Plan 1996-2006, and the NPPF (2019).

10. Existing trees shown to be retained along/adjacent to the boundaries of the site shall

not be lopped or felled and the ground within the root protection areas of any trees shall not be altered or otherwise affected in any way. Trees, hedgerows and groups of mature shrubs adjacent/close to the site shall be retained and protected in accordance with British Standard 5837:2012 'Trees in Relation To Construction Recommendations' (or any subsequent revision) and shall be maintained at all times, until the completion of all building operations on the site.

REASON: To ensure existing trees adjoining the site are not damaged, in the interest of the visual amenity and setting of the area in accordance with policy NBE2 of the adopted Hart Local Plan - Strategy and Sites 2016-2032, saved policies GEN1 and CON8 of the Hart District Local Plan (Replacement) 1996-2006, and the NPPF 2019.

11. Notwithstanding the information submitted with this application, the first-floor window located in the west facing side elevation of the dwelling at plot 1 shall be non-opening and contain frosted glass (Pilkington Glass Level 3 or above, or equivalent) for a height of 1.7m which is measured upwards from the internal floor level their serve. They shall be retained as such, thereafter.

REASON: To protect residential amenity of neighbouring occupiers and to satisfy policy NBE9 of the adopted Hart Local Plan - Strategy and Sites 2016-2032 and saved policy GEN1 of the Hart District Council Local Plan (Replacement) 1996 – 2006.

INFORMATIVES

- 1 The Council works positively and proactively on development proposals to deliver sustainable development in accordance with the NPPF. In this instance, the applicant was advised of the necessary information needed to process the application and once received, further engagement with the applicant was required and the application was subsequently acceptable.
- 2 You may require Building Regulations Consent and we advise that you should contact Building Control on 01252 398715.
- 3 The applicant is advised that should the installation of services conflict with trees and root protection areas, the Tree Section of the Council would have to be approached to discuss the means of installation.